

**UPGRADE OF JENNINGS ROAD, INKOSI
LANGALIBALELE LOCAL MUNICIPALITY, KWAZULU-
NATAL**

Phase 1 Heritage Impact Assessment

October 2017

**Client: Fuze Environmental Services
Nokuthula Nuyswa**

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EXECUTIVE SUMMARY

The Inkosi Langalibabele Local Municipality that falls within the uThukela District Municipality proposes the upgrade of Jennings Road which is situated in Ward 10 of the Local Municipality. The nearest business centre is Estcourt.

This report serves as the Phase 1 Heritage Impact Assessment (HIA) for the proposed upgrade of Jennings Road.

The length of the upgrade is approximately 1321 m in length hence it triggers section 38 (1) (a) of the National Heritage Resources Act (NHRA), 1999 (Act No 25 of 1999) which refers to the construction of a road, wall, power line, pipeline, canal or other similar form of linear development or barrier exceeding 300m in length.

Jennings Road is located in Ward 10 on the north-eastern outskirts of Estcourt just off the R103 / Harding Road. The start of the upgrade is at: 28°59'43.96"S; 29°52'23.57"E and the end is at: 28°59'24.77"S; 29°52'23.44"E. A site inspection of the project area was undertaken on 6 October 2017. Jennings Road is situated in a suburban residential area. The road is potholed and some sections are in a poor condition. Visibility during the site inspection was good.

The specialist understands that no residences / structures will be impacted by the proposed upgrade of Jennings Road.

The length of road to be upgraded was inspected on foot and by vehicle. There are residences on either side of the road for much of the road. A resident said that to her knowledge the suburb was established approx. 46 years ago and previous to this it used to be plots.

Several of the houses have corrugated iron roofs and one of the houses was found to be made from stone and could be older than 60 years. The dwelling is dilapidated and much altered. As the road turns to the north-east, it crosses a watercourse and vacant land. The area is highly disturbed with an existing pipeline as well as with large-scale dumping of litter. It is infested with invasive plant species. No heritage sites were found in this area during the site inspection.

The section from where the road turns to the north-east is untarred and in poor condition. There are houses on either side this section of road as well as power lines. Most of the houses appear to be fairly recently built. The section of Jennings Road in the more recently developed area with low-incoming housing is also untarred. A section of the road runs next to undeveloped land. According to a resident there are no graves or other heritage sites on this vacant piece of land.

No heritage sites were found during the site inspection. This is unsurprising as the Jennings Road environment is highly disturbed by residential activities.

The South African fossil sensitivity map indicates that the project area is situated in an area of very high palaeontological / fossil sensitivity as well as an area of moderate fossil sensitivity. An area of very high fossil sensitivity requires an on-site field assessment and an area of moderate fossil sensitivity requires a desktop palaeontological assessment. However, due to the highly disturbed environment of Jennings Road it is recommended no further palaeontological studies are needed for this project.

From a heritage perspective, the upgrading of Jennings Road can take place as long as the mitigation measures provided in Chapter 9 of the report are implemented and adhered to.

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1. INTRODUCTION

The Inkosi Langalibabele Local Municipality that falls within the uThukela District Municipality proposes the upgrade of Jennings Road which is situated in Ward 10 of the Local Municipality. The nearest business centre is Estcourt.

This report serves as the Phase 1 Heritage Impact Assessment (HIA) for the proposed upgrade of Jennings Road.

2. LEGISLATIVE BACKGROUND

The length of Jennings Road that is to be upgraded is approximately 1321 m in length hence it triggers section 38 (1) (a) of the National Heritage Resources Act (NHRA), 1999 (Act No 25 of 1999), which refers to the construction of a road, wall, power line, pipeline, canal or other similar form of linear development or barrier exceeding 300m in length.

In addition, the proposed upgrade could impact on graves, structures, archaeological and palaeontological resources that are protected in terms of sections 33, 34, 35, and 36 of the KwaZulu-Natal Heritage Act (No. 4 of 2008) as well as sections 34, 35, and 36 of the NHRA.

In terms of Section 3 of the NHRA, heritage resources are described as follows:

- (a) places, buildings, structures and equipment of cultural significance;
- (b) places to which oral traditions are attached or which are associated with living heritage;
- (c) historical settlements and townscapes;
- (d) landscapes and natural features of cultural significance;
- (e) geological sites of scientific *or* cultural importance;
- (f) archaeological and paleontological sites;
- (g) graves and burial grounds, including—
 - (i) ancestral graves;
 - (ii) royal graves and graves of traditional leaders;
 - (iii) graves of victims of conflict;
 - (iv) graves of individuals designated by the Minister by notice in the *Gazette*;
 - (v) historical graves and cemeteries; and
 - (vi) other human remains which are not covered in terms of the Human Tissue Act, 1983 (Act No. 65 of 1983);
- (h) sites of significance relating to the history of slavery in South Africa;

(i) movable objects, including:

- (i) objects recovered from the soil or waters of South Africa, including archaeological and palaeontological objects and material, meteorites and rare geological specimens;
- (ii) objects to which oral traditions are attached or which are associated with living heritage;
- (iii) ethnographic art and objects;
- (iv) military objects;
- (v) objects of decorative or fine art;
- (vi) objects of scientific or technological interest; and
- (vii) books, records, documents, photographic positives and negatives, graphic, film or video material or sound recordings, excluding those that are public records as defined in section 1(xiv) of the National Archives of South Africa Act, 1996 (Act No. 43 of 1996).

The Phase I HIA was undertaken to assess whether any heritage resources will be impacted by the proposed upgrade of the road.

3. LOCATION

Jennings Road is located in Ward 10 of the Inkosi Langalibabele Local Municipality. Ward 10 is situated on the north-eastern outskirts of Estcourt just off the R103 / Harding Road. The start of the upgrade is at: 28°59'43.96"S; 29°52'23.57"E and the end of the upgrade is at: 28°59'24.77"S; 29°52'23.44"E.

4. TERMS OF REFERENCE

Undertake a Phase 1 Heritage Impact Assessment in order to determine the possible existence of archaeological, palaeontological and cultural-historical sites or features in the project area that could be impacted by the proposed road upgrade.

Provide mitigation measures to limit or avoid the impact of the road upgrade on heritage resources (if any).



Figure 1: Road upgrade indicated in cerise

5. METHODOLOGY AND ASSUMPTIONS

A survey of literature was undertaken of the project area in order to place the project in a historical context and to establish what heritage resources might have been identified in the immediate and wider project area during previous heritage assessments.

A site inspection of the project area was undertaken on the 6th of October 2017. Jennings Road is situated in a typical suburban residential area. The road is potholed and some sections are in a poor condition especially the section in the newly developed area featuring low cost housing. Visibility was good during the site inspection.

This Phase 1 HIA report will be submitted to the heritage authority of KwaZulu-Natal, Amafa aKwaZulu-Natali (Amafa), for their assessment and comment.

The specialist understands that no residences / structures will be impacted by the proposed upgrade of Jennings Road.

6. HISTORICAL BACKGROUND OF THE PROJECT AREA

The greater Estcourt area has been relatively well surveyed for archaeological sites in the past. The low altitude and densely wooded areas in the immediate vicinity of Estcourt have been occupied by Middle and later Iron Age farmers since around 1200 AD. The available evidence in the KwaZulu-Natal Museum heritage site inventories, indicates that the area in the vicinity to the study area contains a wide spectrum of archaeological sites covering different time-periods and cultural traditions. These include 11 Early Stone Age site, 28 Middle Stone Age sites, 14 Later Stone Age sites, and 48 Later Iron Age sites (including some Middle Iron Age Sites belonging to the Moor Park Tradition) (Prins 2013:2). Moor Park is a large Middle Iron Age site which was one of a series built on spurs or isolated hilltops with stone walling supplementing the natural topographical defences. It represents a distinct departure from Early Iron Age sites in terms of construction, choice of situation and environment and are the earliest evidence of Iron Age penetration into pure grassland areas (Maggs 1989:37).

Saailaer, also Zaaylager on the outskirts of Estcourt and approximately 2 km south-east of Jennings Road, was the camp of the Voortrekker leader, Gert Maritz. It was the southernmost of the Voortrekker laagers and situated in a horseshoe bend on the Bushman's River. It was

attacked by Zulu warriors on 17 February 1838 a day after the attack on the Voortrekker camp at Bloukrans. The Maritz camp managed to repulse the attack (Derwent 2006:11). A monument to Maritz can be found in the area.

Fort Durnford was built in 1847 on a flat topped hill near Estcourt. It was built by the British to protect the Voortrekkers from Bushmen [cattle] rustlers. It was rebuilt in 1874 or 1875 and was named after Colonel A.W. Durnford, the officer sent in pursuit of Chief Langalibalele after the amaHlubi defied a British order to register their guns (Derwent: 20). A trading store, blacksmith's and inn were initially established and in 1863 it was decided to name the village which had developed around the initial buildings. It was named after Thomas Estcourt, an English parliamentarian who had sponsored the immigration of settlers to the area (Bulpin 1986:487).

In the early stages of the Anglo-Boer War, 1899-1902, the town was garrisoned by about 150 British soldiers which ultimately grew to over 5000 troops as the war progressed. Armoured reconnaissance trains were pushed northwards from Estcourt station towards Colenso. The town was an important staging post for troops involved in the attempts to relieve the siege of Ladysmith (Jones & Jones 1999:74).

The 1:50 000 (2829DD) map indicates that the northern section or half of Jennings road had not yet been developed in the year 2000 when the map was compiled (see **Figure 2** below).

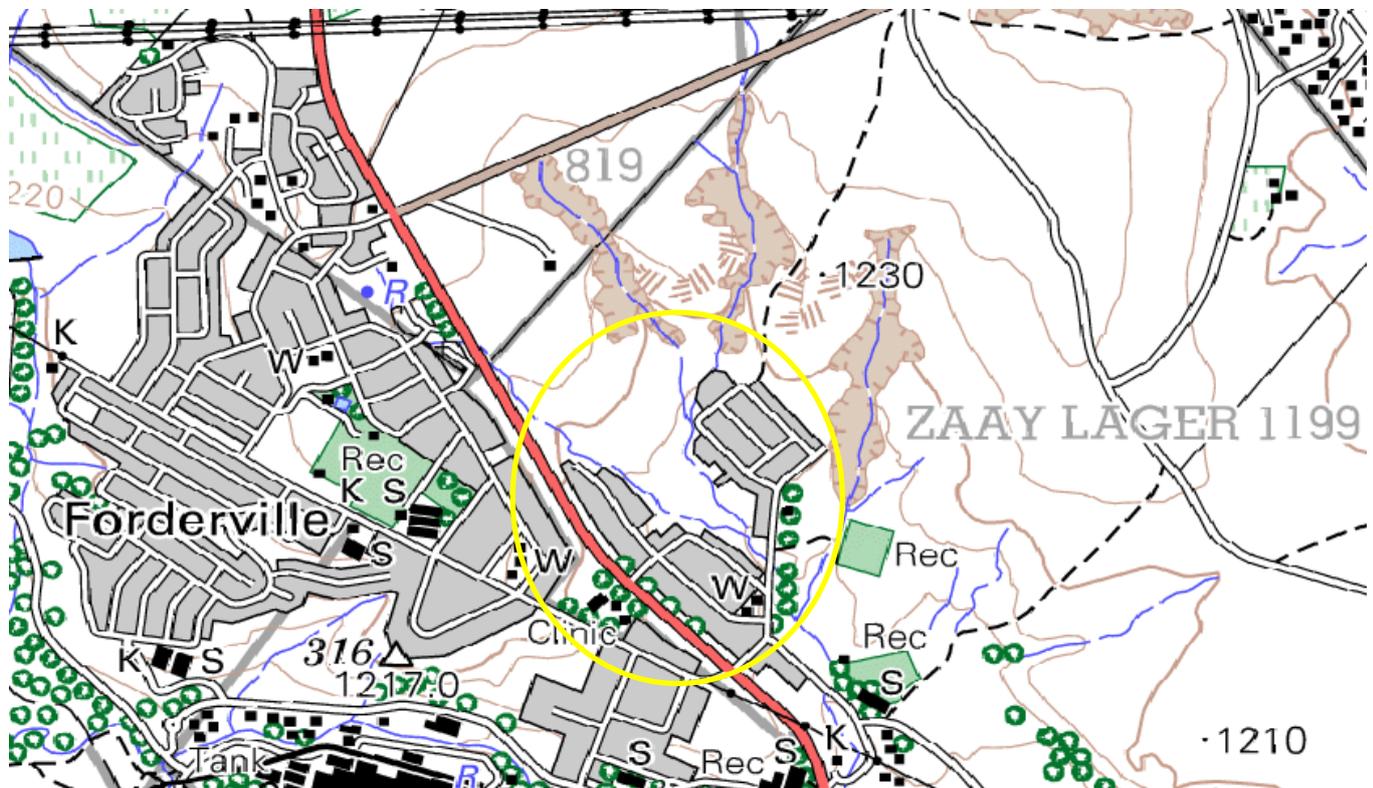


Figure 2: 2829DD map with project area indicated with yellow circle

7. RESULTS OF SITE INSPECTION

The length of road to be upgraded was inspected on foot and by vehicle. There are residences on either side of the road for much of the road. The specialist spoke to a few residents regarding the area.

Reverend Ruth James, a resident, said that the suburb is called Colita and it was called this name because when it was developed people were asked to suggest names for the suburb and some people said 'call it this' and some said 'call it that' hence the name Colita. She also said that to her knowledge it was established approximately 46 years ago and the area used to be agricultural plots before the suburb was developed.



Figure 3: Looking east towards intersection with Canna Avenue



Figure 4: Looking up Jennings road in a north-west direction

Several of the residences have corrugated iron roofs. The house pictured below has corrugated roofing as well as fencing with wagon wheels depicted thereon which was a popular design in the 1970s.



Figure 5: Corrugated iron roof house

A house made from stone (possibly No. 16 or No. 20 Jennings Road) appears to be older than 60 years. It is, however, understood that no structures will be impacted by the proposed upgrade

of the road. The dwelling is dilapidated and much altered but the original stone work can still be seen in **Figure 6**.



Figure 6: Structure made from stone



Figure 7: View down Jennings Road

As the road turns to the north-east, it crosses a watercourse and vacant land. This area is highly disturbed with an existing pipeline as well as large-scale dumping of litter taking place. It is

infested with invasive plant species. No heritage sites were found in this area during the site inspection.



Figure 8: Overgrown watercourse

The section from where the road turns to the north-east is untarred and in poor condition. There are houses on either side this section of road as well as power lines. Most of the residences / houses appear to be fairly recently built.



Figure 9: Untarred section of Jennings Road



Figure 10: Untarred section looking in a south-westerly direction

The section of Jennings Road in the more recently developed area where low-incoming housing has been built is also untarred. A section of the road runs next to undeveloped land as seen in **Figure 11** below.



Figure 11: Untarred extension of Jennings Road

Mr Leon Adams, who identified himself as a community leader, told the specialist that, to his knowledge, there are no graves nor other heritage sites on the vacant land. A house is in the

process of being built on this parcel of land some distance from the gravel road as can be seen in **Figure 12**. No heritage resources were found on the vacant land.



Figure 12: Vacant land with a house being built in background.

No heritage sites were found during the site inspection. The Jennings Road environment is highly disturbed by residential activities and no heritage sites were found in the few vacant areas along Jennings Road.

The South African Fossil Sensitivity Map indicates that the project area is situated in an area coloured in red that indicates an area of very high palaeontological / fossil sensitivity (see **Figure 13** below) as well as an area of moderate fossil sensitivity (indicated in green). As indicated in the legend in the figure below, an area of very high fossil sensitivity requires an on-site field assessment and an area of moderate fossil sensitivity requires a desktop palaeontological assessment. Due to the highly disturbed environment of Jennings Road it is recommended no further palaeontological studies are needed for this project.

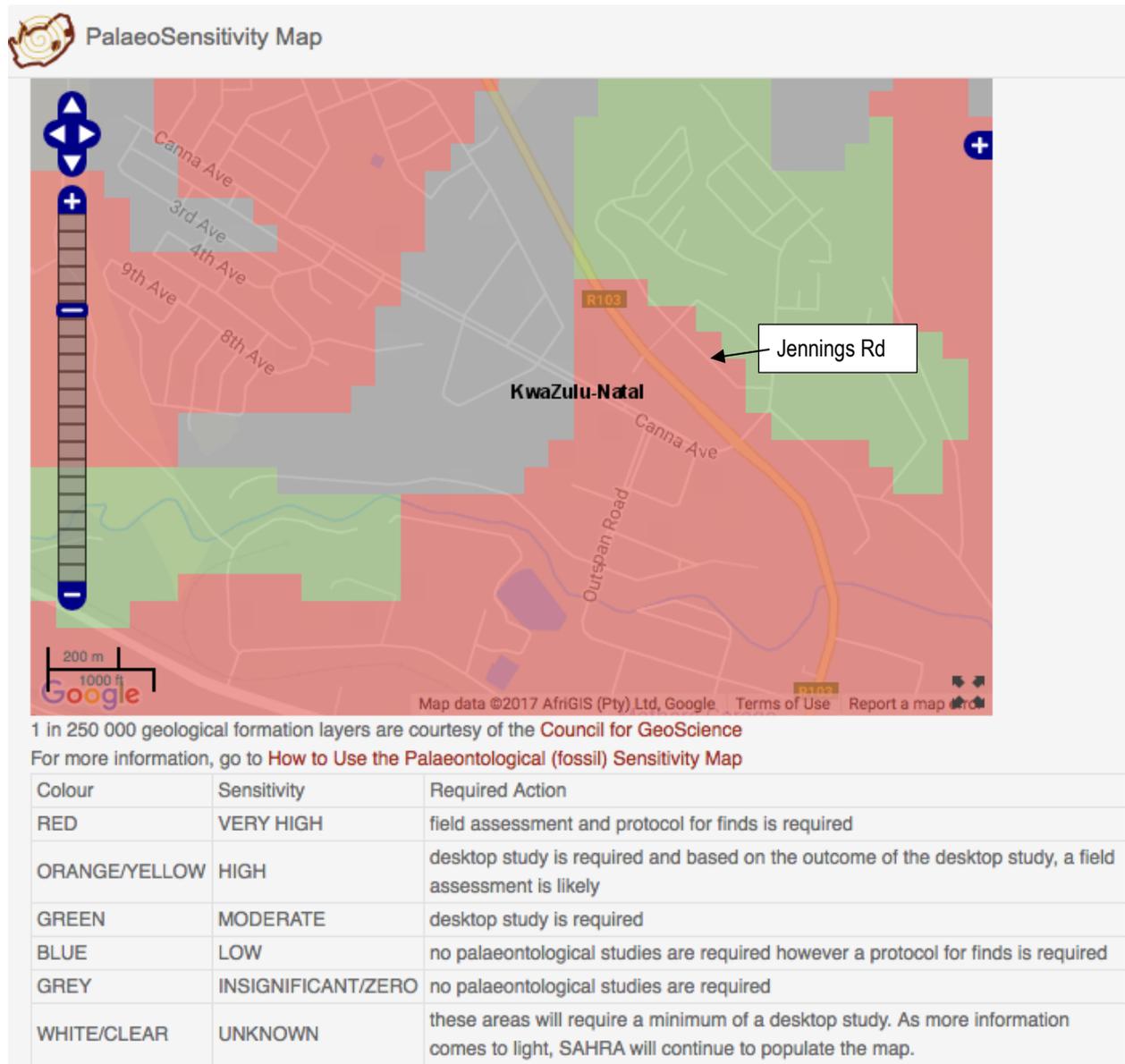


Figure 13: Fossil sensitivity of Jennings Road

8. CONCLUSION

No heritage resources were found along Jennings Road including the vacant areas adjacent to the road. Due to the high degree of disturbance along and around Jennings Road, it is recommended that no further palaeontological studies are required.

From a heritage perspective, the upgrading of Jennings Road can take place as long as the mitigation measures provided below are implemented and adhered to.

9. MITIGATION MEASURES

- For any chance finds of heritage resources, such as graves or archaeological residues, all work must cease in the area affected and the Contractor must immediately inform the Project Manager. A registered heritage specialist must be called to site for inspection. The relevant heritage resource agency (Amafa) must also be informed about the finding.
- The heritage specialist will assess the significance of the resource and provide guidance on the way forward.
- Written permission (permits) must be obtained from Amafa if heritage resources are to be altered, destroyed or removed.
- All heritage resources found in close proximity to the construction area must be protected by a 5m buffer in which no construction can take place. The buffer material (danger tape, fencing, etc.) must be highly visible to construction crews.
- Under no circumstances may any heritage material be destroyed or removed from site unless under direction of a heritage specialist.
- Should any remains be found on site that are potentially human remains, the South African Police Service (SAPS) should also be contacted. No SAPS official may disturb or exhume such remains, whether of recent origin or not, without the necessary permission.
- The following should be followed in terms of chance fossil finds:
 - When the upgrade of the road begins, rocks must be given a cursory inspection by the environmental officer or designated person. Any fossiliferous material (trace fossils, plants, insects, bone, and coal) should be put aside in a suitably protected place. This way the upgrading of the road will not be interrupted.
 - Photographs of possible fossils should be sent to a palaeontologist for preliminary assessment.
 - A qualified palaeontologist should visit the site to inspect the selected material and check dumps where feasible. The frequency of inspections should be dependent on the finding of any potentially important fossil material.
 - Fossil plants or vertebrates that are considered to be of good quality or scientific interest by the palaeontologist must be removed, catalogued and housed in a suitable institution where they can be made available for further study. Before the fossils are removed from the site an Amafa / SAHRA permit must be obtained. Annual reports must be submitted to Amafa and SAHRA as required by the relevant permits.
 - If no fossils are found and the upgrade of the road is finished then no further monitoring is required.

10. REFERENCES

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