

DINING CARS – Diagram A

General information:

A total of 93 Dining cars were built during the period 1897 to 1938 with numbers 101 to 341. Several of these dining cars were upgraded and then re classified and renumbered.

A very large variety of dining cars was manufactured and represents 36 variations namely Type :A-1 / A- 2/ A-3 / A-4 / A-5/ A-6 / A-7 / A-8 / A- 9 / A-10 / A-11 / A- 12/ A-13 / A -14/ A-15 / A-16 / A-17 / A-18 series 1 / A -18 series 2 / A-18 –T / A-19 / A-20 / A- 21 / A-22 / A-24 / A-26 / A- 27 / A-28 /A-30/ A- 31 / A-33 / A-35 / A-36 /A-37 / A-40 / A-41.

Although not identical, Dining car No 234- Type 28, is represented by the following Dining cars that are preserved and /or in operation.

Preserved Rolling stock:- Transnet

NO	Name	Type	Building date	Location	Condition
134	Kafue	A-12	1906	Museum George	Good
168	Palala	A – 18 (2)	1914	Museum , George	Good
196	Tewani	A - 22	1924	Museum, George	Good
199	Umfolozu	A - 22	1924	Museum, George	Good
230	Orange	A - 33	1940	Epping/ Loan Bay Steamers	Fair

Transnet asset on loan / Private Ownership

NO	Name	Type	Building date	Owner	Condition
198	Umgenei	A - 22	1924	Sandstone	Unknown
215	Kowie	A - 22	1928	Sandstone	Unknown
218	Riet	A - 22	1928	Sandstone	Unknown
148	Pafuri	A - 22	1926	Rovos Rail	Unknown
195	Shangani	A - 22	1924	Rovos Rail	Unknown
197	Letaba	A - 22	1924	Rovos Rail	Unknown
205	Umdlali	A - 22	1924	Rovos Rail	Unknown
211	Wembley	A - 22	1926	Rovos Rail	Unknown
220	Kei	A - 28	1936	Rovos Rail	Unknown
225	Nile	A - 28	1936	Rovos Rail	Unknown
226	Modder	A - 28	1937	Rovos Rail	Unknown
231	Zambezi	A – 33	1940	Rovos Rail	Unknown
232	Umvoti	A – 28	1937	Rovos Rail	Unknown
235	Nyanza	A – 28	1939	Rovos Rail	Unknown
237	Mazoe	A - 31	1942	Rovos Rail	Unknown
217	Selati	A – 22	1927	Ceres Railway	Unknown
243	Umtata	A - 31	1942	Ceres Railway	Unknown
127	Umlzumbi	A - 11	1905	Transnet /loan	Unknown
157	Umfuili	A - 18	1912	Umgenei steam	Unknown
209	Maputo	A - 22	1930	Umgenei steam	Unknown
233	Mooi	A - 28	1937	Umgenei steam	Unknown

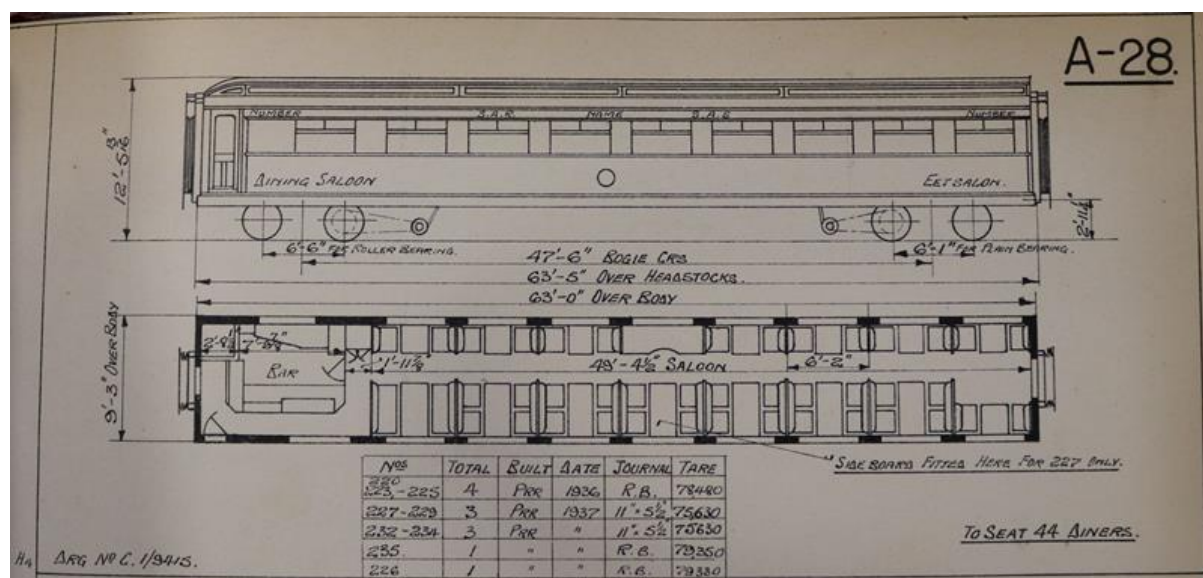
Motivation for disposal:-

Coach No 234 is in a very poor condition and the body of the coach has to be rebuilt due to years of exposure to the weather conditions. Over the years the interior has also been vandalised to such an

Official list of Dining Cars.

DIAGRAM A-DINING CARS.										
Nº	NAME	KITCHEN CAR	ALLEGRA	Nº	NAME	KITCHEN CAR	ALLEGRA	Nº	NAME	KITCHEN CAR
127.	UMZUMBI		A-11.	178.	MAHOON		A-18.	223.	TOISE	278.
128.	UMZINTO		A-11.	179.	ADA JUBA		A-18.	224.	SWYKA	279.
129.	MATOPPO		A-12.	180.	MATROOSBERG		A-18.	225.	NILE	280.
130.	TRAKA		A-12.	181.	SEBAKWE		A-18.	226.	MOSEER	281.
131.	HEX		A-26.	182.	UMLABO		A-18.	227.	KIESKAMA	282.
132.	BASHEE		A-26.	183.	KUMANI		A-18.	228.	MABUSI	283.
133.	OLIFANTS		A-12.	184.	UMSINGA		A-18.	229.	SHASHI	284.
134.	KAFUE		A-12.	185.	ELANDS		A-18.	230.	ORANGE	285.
135.	LIMPOPO		A-12.	186.	UMKAZI		A-18.	231.	ZAMBESI	286.
136.	MARICO		A-13.	187.	PVANA		A-18.	232.	UMVOTI	287.
137.	SABIE		A-13.	188.	SGEGE		A-18.	233.	MOOI	288.
138.	MAGALIES		A-13.	189.	WILDEGEEST		A-21.	234.	UMKONKRAAS	289.
139.	COLGA		A-14.	190.	GOURITZ		A-21.	235.	NYANZA	290.
145.	SELVALIS	263.	A-22.	191.	ARTULA		A-21.	236.		
146.	PONGOLA	264.	A-22.	195.	SHANGANI	250.	A-22.	237.		
148.	PAFURI	265.	A-22.	196.	TEWANI	251.	A-22.	238.		
151.	SHIRE		A-18.	197.	LETABA	252.	A-22.	239.		
153.	TANGANYIKA		A-18.	198.	UMGENTI	253.	A-22.	240.		
154.	NGAMI		A-18.	199.	UMFOLOSI	254.	A-22.	241.		
155.	KHAMI		A-18.	200.	ASSEGANI	255.	A-22.	242.		
156.	ZIMBABWE		A-18.	201.	NOTWANI	256.	A-22.	243.		
157.	UMFULI		A-18.	202.	UMARUKO	257.	A-22.	244.		
158.	COMPASSBERG		A-18.	204.	KUNENE	259.	A-22.	245.		
159.	OUTENIKWA		A-18.	205.	UMHLALI	260.	A-22.	246.		
160.	PUTOVALE		A-18.	206.	LUNDI	261.	A-22.	247.		
161.	PHANTOM PASS		A-18.	207.	GAMTOOS		A-18.	248.		
162.	CHUMULA		A-18.	208.	SINGWEEZI	273.	A-22.	249.		
165.	UMSINSUSI		A-18.	209.	MAFUTO	274.	A-22.	341.	VOORTREKKER	
166.	ILLOVO		A-18.	210.	UMGWEZI	275.	A-22.	351.		
167.	LIESBECK		A-18.	211.	WEMBELEY	262.	A-22.	352.		
168.	PALALA		A-18.	215.	VAAL	267.	A-22.	353.		
169.	LETSANI		A-18.	214.	TUGELA	268.	A-22.	354.		
170.	BUFFALO		A-18.	215.	KOWIE	269.	A-23.	355.		
171.	CROCODILE		A-18.	216.	CALEDON	270.	A-22.			
173.	AMANTZIMOTI		A-18.	217.	SELATI	271.	A-22.			
174.	SENYATI		A-18.	218.	KIET	272.	A-22.	11001.	LUKOSI	A-18-7
175.	ORAKENSBERG		A-18.	219.	PROTER	273.	A-24.	11002.	KATUNGA	A-18-7
177.	UMSILWANA		A-18.	220.	KEI	277.	A-28.	11003.	MATESI	A-18-7

Diagram of Dining car No :-



Information resources:-

S.A.R – Types of Main Line Saloons 3'6" Gauge - Chief Mechanical Engineers – 14 August 1941

S.A.R – Types of Main Line Saloons 3'6" Gauge - Chief Mechanical Engineers – 01 November 1980 (updated)

Railway Dining Cars in South Africa – History and Development by H.L Pivnic

Classification of the various Dining Cars is recorded in above publications reflecting the Diagram (coach layout) , Type, Numbers of the dining cars, builders name and building date. All publications are available at the Transnet Transport Museum in George while duplicate publications and the history of the coaches are preserved for research purposes at the Transnet Knowledge Centre in Johannesburg.

